

**423.0 Km's: Mitchell Highway onto Manildra Street at Narromine.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/WPhQahdAKGjRSNvy8>

**COMPANY PILOT 1:** Warn all traffic 150 Metres in front of the load.

**COMPANY PILOT 2:** Warn all traffic 100 Metres in front of the load.

**COMPANY PILOT 3:** Warn all traffic 50 Metres in front of the load.

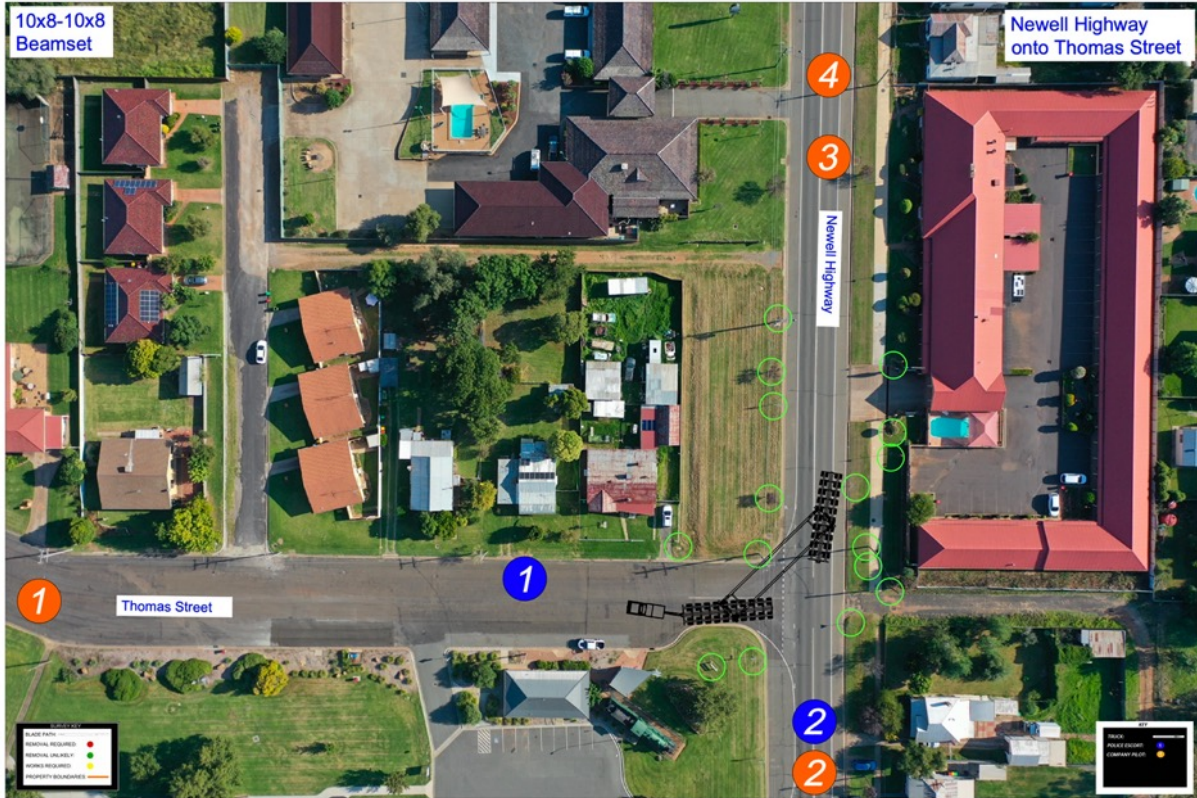
**COMPANY PILOT 4:** Stay 500 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn left from the Mitchell Highway onto Manildra Street before crossing onto Algalah Street while crossing onto the incorrect side of the road on several intersections.

**ROAD MODIFICATIONS:** Nil.

**525.0 Km's: Newell Highway onto Thomas Street at Parkes.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/fSnFVWPr78RePSTz9>

**COMPANY PILOT 1:** Warn all eastbound traffic on Thomas Street.

**COMPANY PILOT 2:** Warn all northbound traffic on the Newell Highway.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from the Newell Highway onto Thomas Street while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**527.4 Km's: Moulden Street onto Back Trundle Road onto Ross Road at Parkes.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/1JA2hh5EMZ3G3Qfs9>

**COMPANY PILOT 1:** Warn all northbound traffic on Ross Road.

**COMPANY PILOT 2:** Warn all northbound traffic on Ross Road.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right than left from Moulden Street onto Ross Road via Back Trundle Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**527.8 Km's: Ross Road onto Henry Parkes Way at Parkes.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/UKvs2QekwwFuP6oS8>

**COMPANY PILOT 1:** Warn all westbound traffic on Henry Parkes Way.

**COMPANY PILOT 2:** Warn all eastbound traffic on Henry Parkes Way.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn left from Ross Road onto Henry Parkes Way.

**ROAD MODIFICATIONS:** Nil.

**528.0 Km's: Henry Parkes Way onto Westlime Road at Parkes.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/QhZmTtpL1vNkUE7i8>

**COMPANY PILOT 1:** Warn all northbound traffic on Westlime Road.

**COMPANY PILOT 2:** Warn all westbound traffic on Henry Parkes Way.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from Henry Parkes Way onto Westlime Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**532.0 Km's: Hartigan Avenue onto the Newell Highway at Parkes.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/y3rabftt4HGReX9e6>

**COMPANY PILOT 1:** Warn all northbound traffic on the Newell Highway.

**COMPANY PILOT 2:** Warn all northbound traffic on the Newell Highway.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from Hartigan Avenue onto the Newell Highway while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.



**664.0 Km's: The Newell Highway onto Compton Road at West Wyalong.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/PeM4uWg5hLiyZiPd8>

**COMPANY PILOT 1:** Warn all northbound traffic on Compton Road.

**COMPANY PILOT 2:** Warn all northbound traffic on Compton Road.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

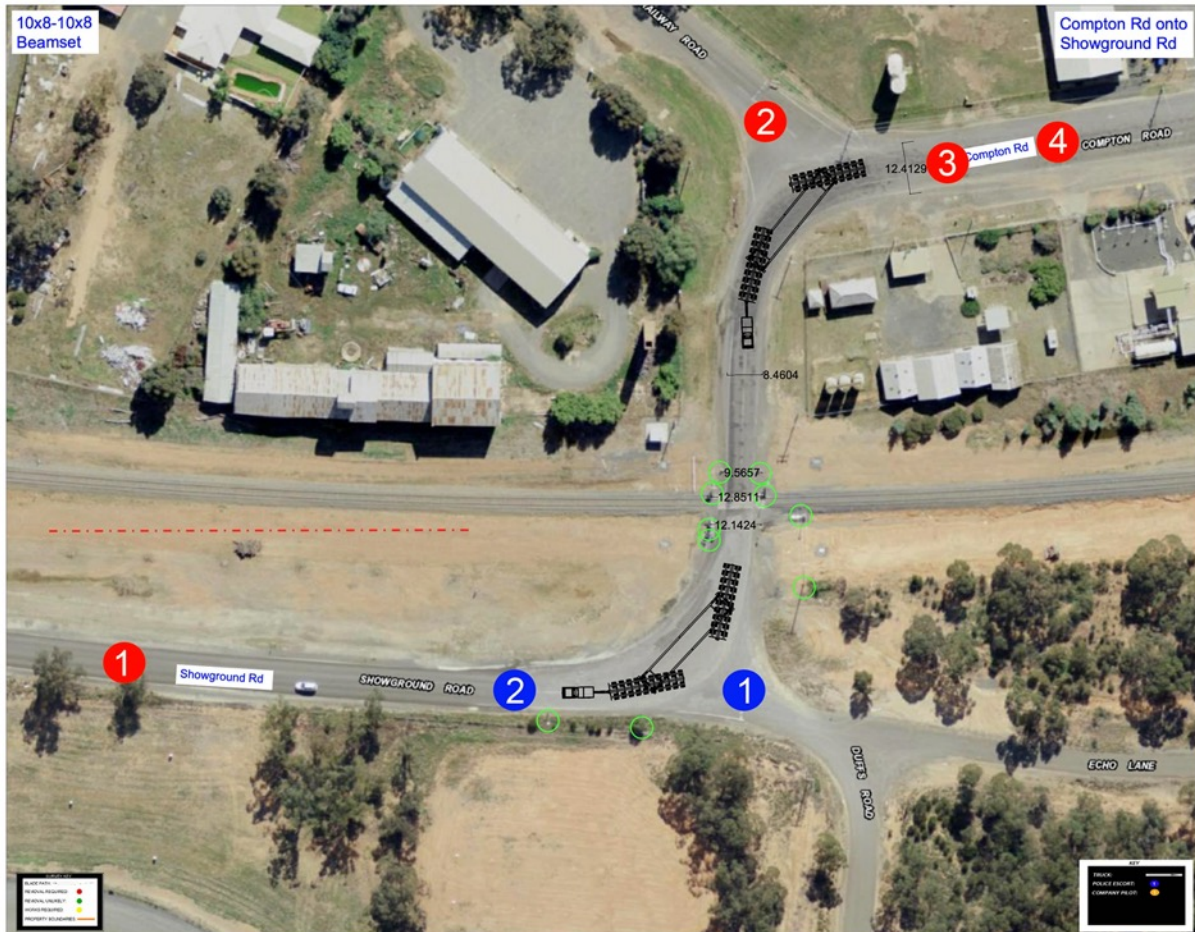
**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn left from the Newell Highway onto Compton Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**666.5 Km's: Compton Road onto Showground Road at West Wyalong.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/hts5qARMMWZcvW7R7>

**COMPANY PILOT 1:** Warn all northbound traffic on Compton Road.

**COMPANY PILOT 2:** Warn all northbound traffic on Compton Road.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Dogleg turn from Compton Road onto Showground Road while crossing onto the incorrect side of the road. Rail protection officer may be required to travel over the rail crossing.

**ROAD MODIFICATIONS:** Nil.

**669.0 Km's: Showground Road onto the Newell Highway at West Wyalong.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/yAyBdrZcocEeTBnz6>

**COMPANY PILOT 1:** Warn all northbound traffic on the Newell Highway.

**COMPANY PILOT 2:** Warn all northbound traffic on the Newell Highway.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn left from Showground Road onto the Newell Highway while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**803.0 Km's: Whitton Street onto Cadell Street at Narrandera.**  
**(Newell Highway)**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/EXcuuBeMsXdhVDtm8>

**COMPANY PILOT 1:** Warn all northbound traffic on Caddell Street.

**COMPANY PILOT 2:** Warn all northbound traffic on Caddell Street.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn left from Whitton Street onto Caddell Street while staying on the correct side of the road.

**ROAD MODIFICATIONS:** Nil.

**825.0 Km's: The Sturt Highway onto Sandigo Road at Sandigo.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/c3ZDZEj78aMwmqEh8>

**COMPANY PILOT 1:** Warn all northbound traffic on Sandigo Road.

**COMPANY PILOT 2:** Warn all westbound traffic on the Sturt Highway.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from the Sturt Highway onto Sandigo Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**826.0 Km's: Sandigo Road onto Muntz Road at Sandigo.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/WErjz7kPjHWLPRsP6>

**COMPANY PILOT 1:** Warn all eastbound traffic on Muntz Road.

**COMPANY PILOT 2:** Warn all eastbound traffic on Muntz Road.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from Sandigo Road onto Muntz Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

**828.0 Km's: Muntz Road into Avonleigh Solar Access Road at Sandigo.**

Image 1:



**GPS LINK FOR SECTION OF ROAD:** <https://goo.gl/maps/R3T3az3oQiSRg6GV7>

**COMPANY PILOT 1:** Warn all eastbound traffic on Muntz Road.

**COMPANY PILOT 2:** Warn all eastbound traffic on Muntz Road.

**COMPANY PILOT 3:** Stay 50 metres behind the load and warn all traffic.

**COMPANY PILOT 4:** Stay 100 metres behind the load and warn all traffic.

**PINCHPOINT PROCEDURE:** Load to turn right from Muntz Road onto Avonleigh Solar Access Road while crossing onto the incorrect side of the road.

**ROAD MODIFICATIONS:** Nil.

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## 15.0 Pinch Points

The following are the pinch points on this route:

- **REGULAR ROUTE ASSESSMENTS:** Prior to the movement Rex J Andrews P/L is to contact TfNSW and local councils regarding any potential roadworks that road stakeholders would have taken place on the route during the proposed delivery dates. Driver/Supervisors are to have full contact details and communicate with these roadwork's managers while on route.
- **SWEPT PATH ANALYSIS:** This study shows all pinchpoint manoeuvres on route and is a guide to show possible arrangements for traffic control at each of the pinchpoints.
- **BRIDGE CROSSINGS:** The load is to travel over all structures as per the TfNSW bridge assessment for this movement. Load to take special caution at Denman Bridge and lower the load to 5.5 metres maximum before crossing this structure in the centre of both lanes.
- **NEWCASTLE:** The load will need to undertake a U-turn on Maitland Road at Sandgate. All personnel are to be tool boxed on the methodology of these procedures.
- **NARRENDERA:** Some tree pruning is required in Narrendera prior to the load departing Newcastle.

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### **PINCHPOINT PROCEDURES**

Whilst some pinch points are known along the route demonstrating a method of negotiating each individual hazard would be flawed as traffic conditions are constantly changing.

It is crucial that appropriate measures are applied to avoid impact to road users and road infrastructure, the chosen route has been assessed and the load is capable of navigating the route, however local traffic conditions can create pinch points.

A pinch point is an area identified by the lead pilot and relayed to the convoy as having the potential to interfere with the swept path of the load, pinch points can be created by road furnishings, roundabouts, narrow sections of road, roadkill, corners, road works, parked vehicles, damaged pavement, this list is not exhaustive.

For the purposes of this traffic management plan identified pinch points will follow the following protocol.

The lead pilot must travel a sufficient distance in front of the load so as to survey the swept path required for the load, this will allow sufficient time to relay back road conditions or choke points to allow the driver to halt the load before causing congestion to other road users.

In the event of parked vehicles or local traffic conditions preventing the load from safely navigating the permitted route, the load cannot proceed until it is safe to do so.

The lead pilot will warn all oncoming traffic of the impending load, when the way forward for the transporter is established as being clear the load may proceed.

If built up queued traffic is behind the load, ensure that an opportunity to allow this traffic to pass is taken at the first safe opportunity.

The procedure for crossing bridges is reliant on only the load being on the bridge during the crossing, this will require a concentrated effort from the escort team to ensure that all vehicular traffic both in front of and behind the load are warned of the hazard.

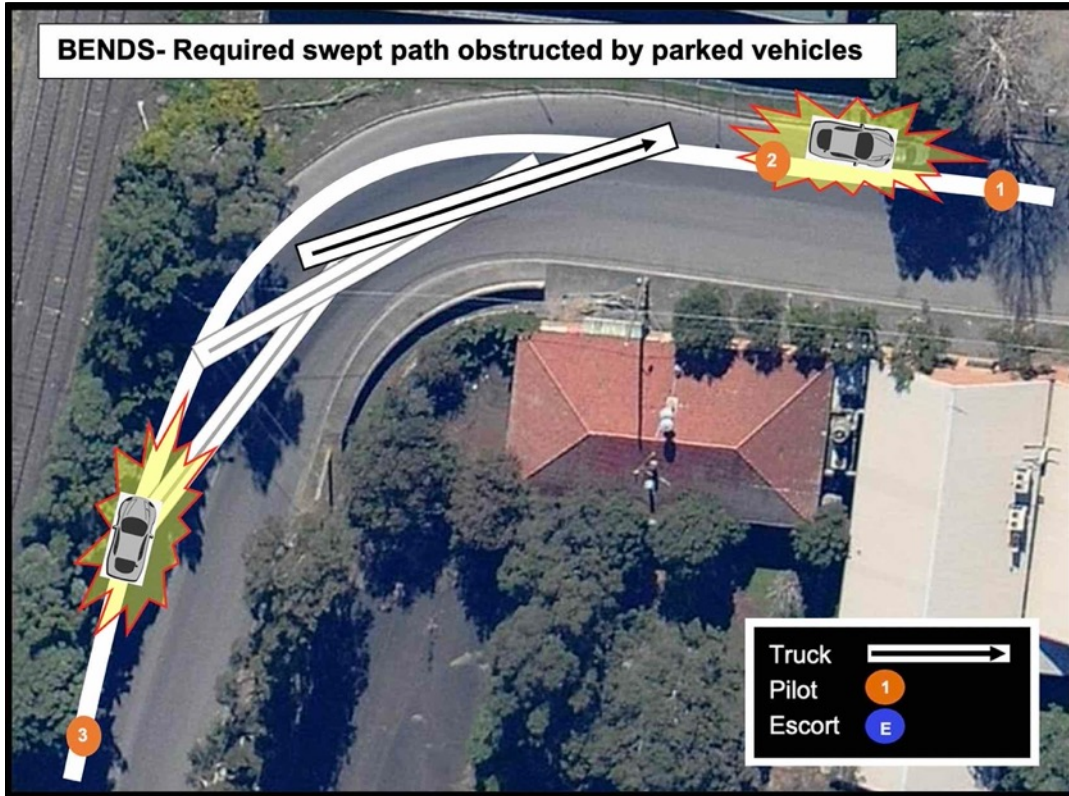
It is crucial that pinch points are discussed at the toolbox briefing and that all parties are aware of the protocols in place.

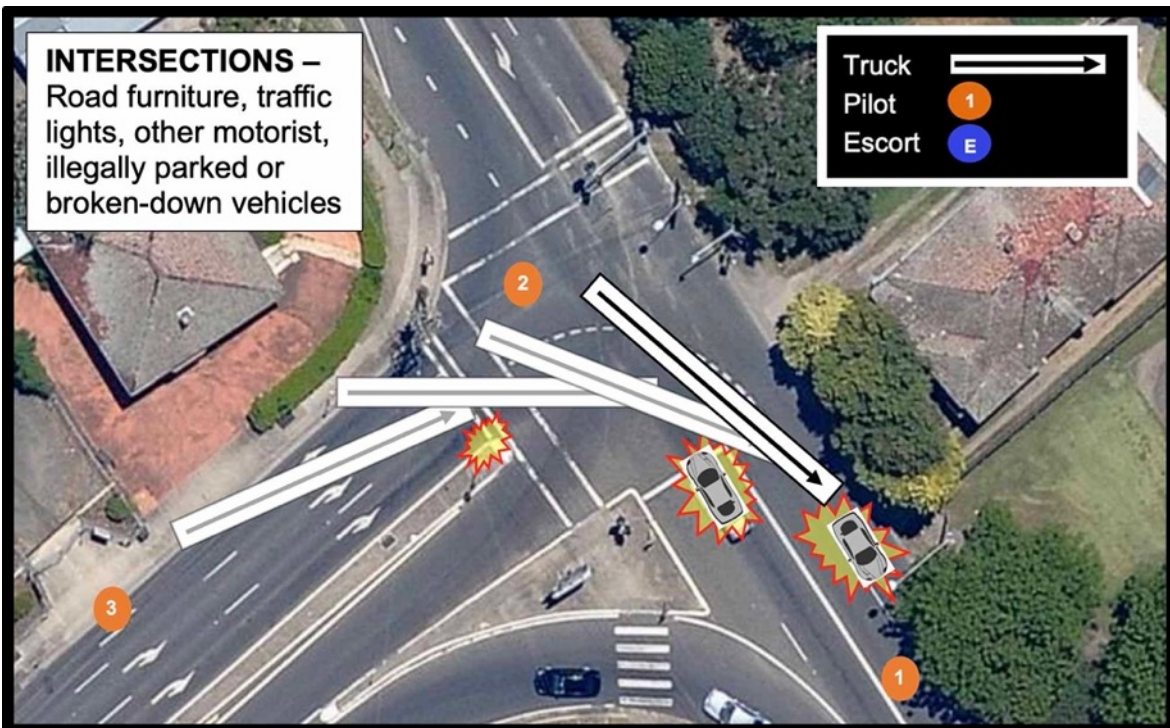
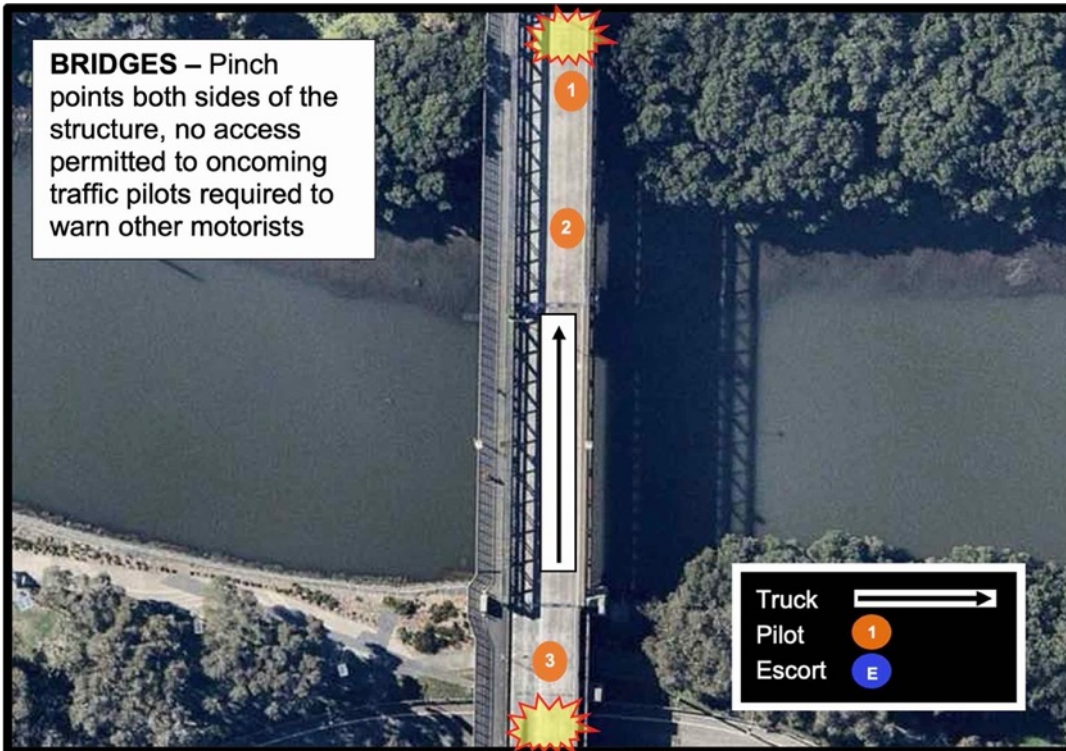
Drivers should familiarise themselves with the route including nominated bypasses for heavy vehicles along the route.

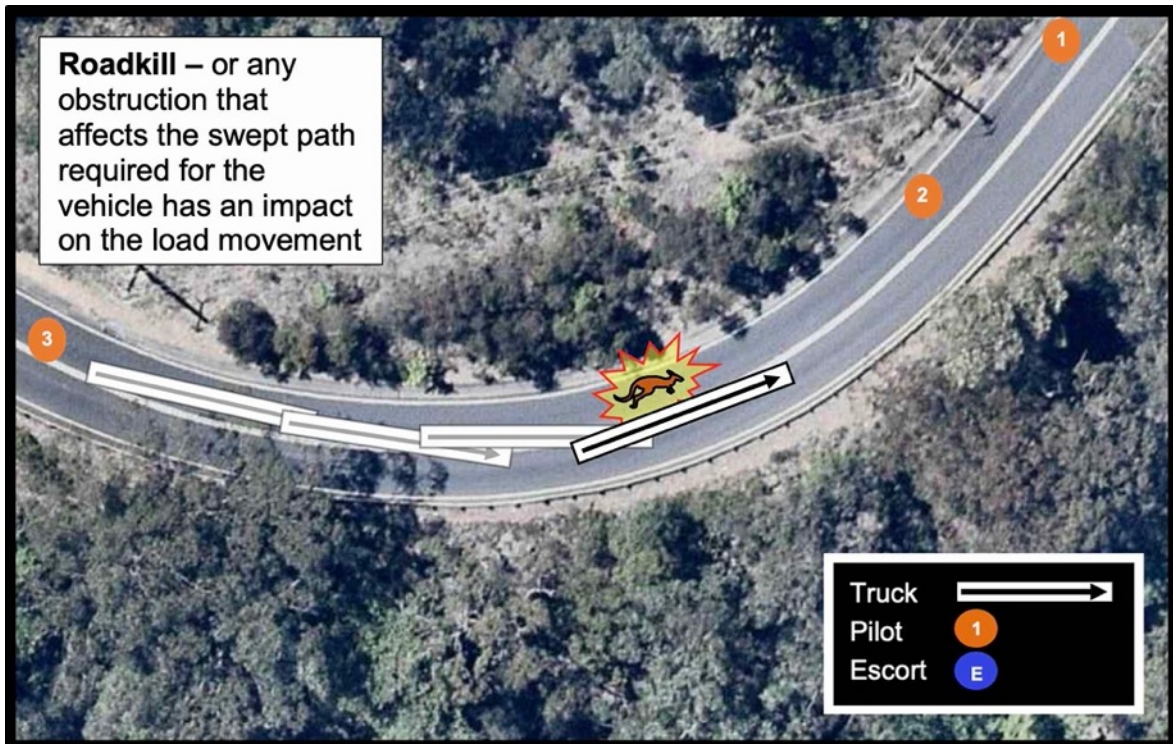
If there is any doubt as to the viability of accessing the permitted route the load must not continue until the way forward has been deemed appropriate.

For more detail analysis of coping with roadwork refer to section 11.

Examples of pinch points:



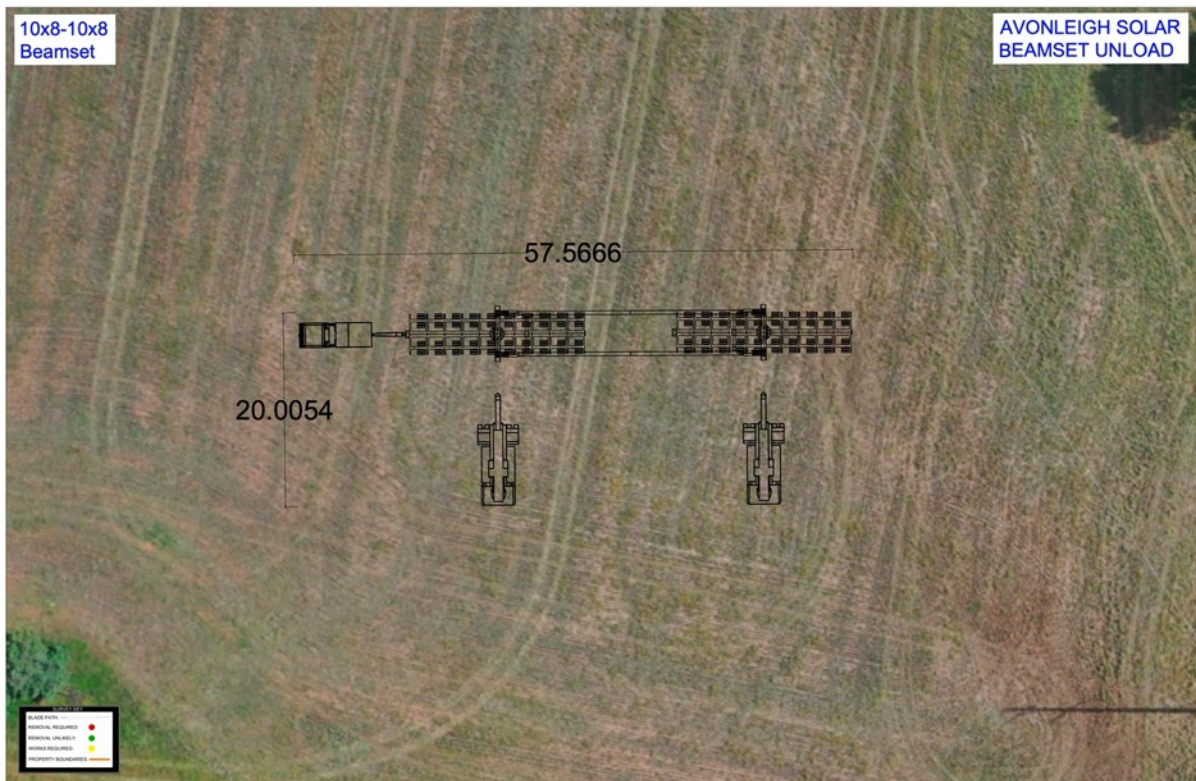




## 16.0 Beamset unload:

The beamset will need to be able to park parallel to the foundation, and enough room provided for 2 x 90T cranes to disassemble the beamset. Please see below image for an idea of the area required to undertake these works.

Image 1:





## 18.0 References:

Australian Load Restraint Guide  
Rex J Andrews P/L transport drawing  
Hyosung transformer drawing  
Rex J Andrews route survey #333 REV00  
Google Earth/Maps  
Nearmaps  
NHVAS Maintenance Management (NHVAS21193)  
NHVAS Basic Fatigue Management (NHVAS21193)



## Appendix F Driver Code of Conduct

The Driver Code of Conduct is to ensure that drivers adhere to safe driving practices. Any non-compliance with the Drivers Code of Conduct will be managed through the Construction Contractors' general business code of conduct.

### 1. General Controls

Drivers must:

- Comply with the Project Traffic Management Plan.
- Hold the appropriate licence class for the vehicle driven.
- Comply with all road laws and regulations.
- Exercise extra care in adverse weather conditions that may affect road safety, such as fog, dust, wet weather and flooding.
- Report any observations of driver fatigue or misconduct to the HSE Coordinator.
- Respect the rights of all road users to share the road.
- Cover loads when transporting loose materials (e.g. aggregate).
- Avoid travelling in convoys or platoons, with all long vehicles (7.5m long or longer) to be driven at least 60m behind another long vehicle, that they are following.
- Report any incidents in accordance with the Work Health and Safety Management Plan.

### 2. Driving to and from the Project

Driving presents a serious risk to health and safety and all precautions are to be taken when driving to and from site. Major driving hazards include mobile phone use, speeding and drug and alcohol usage and fatigue.

#### Mobile Phone Use

The use of mobile phones while operating a motor vehicle is strictly prohibited unless a blue tooth hands-free kit is utilised. Placing mobile phones in 'Do not disturb' mode while driving is encouraged.

#### Travelling Speeds

The existing posted speed limits for Golden Highway, Ringwood Road and Wollar Road is 100 km/h. Barnett Street does not have a posted speed limit, and the default speed limit of 100 km/h would apply. Drivers must:

- Be aware of the legal speed limit.
- Not exceed the legal speed limit on public roads.
- Not exceed the speed limit designated for internal access roads within the Project.
- Adjust speed to suit the road environment and prevailing weather conditions.
- Adjust speed to ensure the safe movements of the vehicle based on the vehicle configuration.

### **Drug and Alcohol Usage:**

The Project has a zero-alcohol tolerance and zero illicit drugs tolerance or impairment by prescription medication.

Drug and alcohol testing will be conducted at random times.

### **3. Driver Fatigue**

Under the Heavy Vehicle Driver Fatigue Reform (2008), all drivers of trucks and truck combinations over 12 tonne GMV (except for Ministerial Exemption Notices that may apply) are required to operate under one of three fatigue management schemes:

- Standard Hours of Operation
- Basic Fatigue Management
- Advanced Fatigue Management.

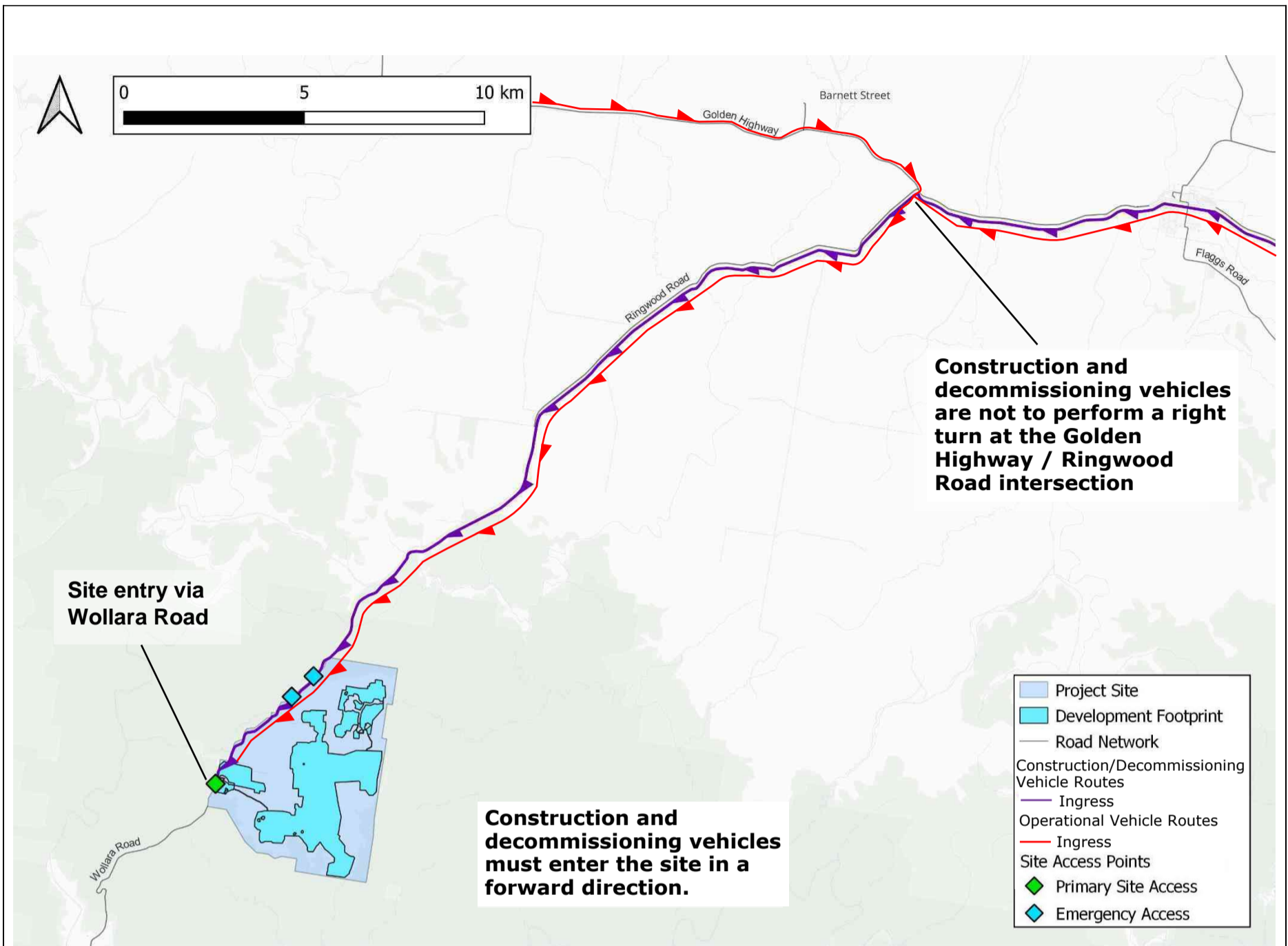
All heavy vehicle operators are to be aware of their adopted fatigue management scheme and operate within its requirements.

### **4. Designated Transport Routes**

Drivers must:

- Travel to the Project via Golden Highway, Ringwood Road, Wollara Road and Barnett Street, with left-in and left-out only permitted at the Golden Highway / Ringwood Road intersection.
- Use only the established Project access point off Wollara Road to enter/exit the Project.
- Use agreed haulage routes as per the VMP (see **Appendix G**) unless an approved alternative route is in place due to an emergency or other unavoidable circumstance.
- Enter and exit the Project in a forward direction.
- Ensure vehicles leaving the Project are in a clean condition.
- Notify the HSE Coordinator immediately if there is any damage to the Project access route which may present a safety hazard.

## Appendix G Vehicle Movement Plan



Construction / decommissioning vehicle routes - ingress:

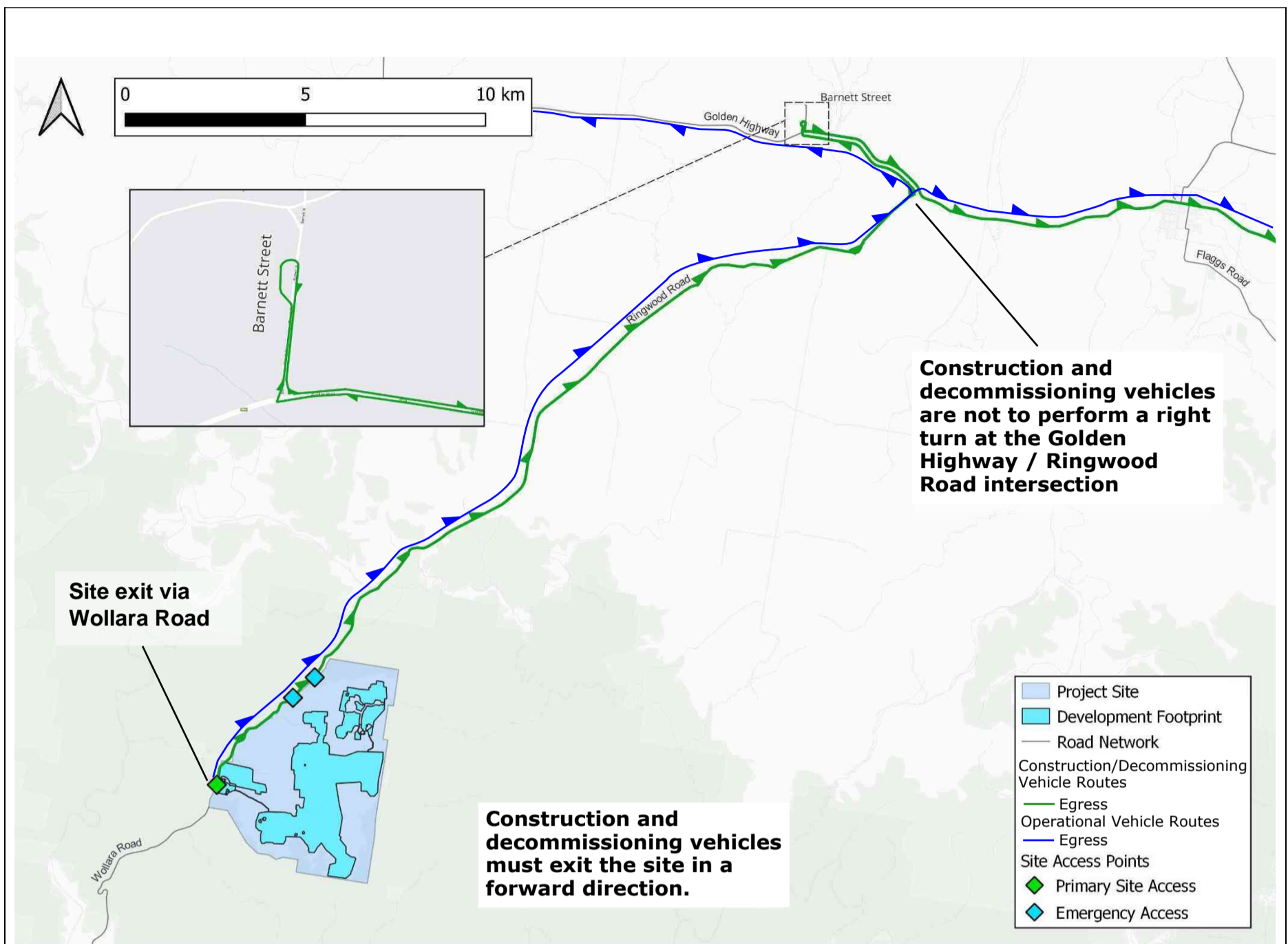
Golden Highway westbound, left turn onto Ringwood Road southbound, continue straight onto Wollara Road southbound and left-turn into the site.

Operational vehicle routes - ingress:

Vehicles can undertake all legal movements associated to their vehicle type.

**VEHICLE MOVEMENT PLAN – GOULBURN RIVER SOLAR FARM  
SITE ENTRY / INGRESS**

**Adherence to this VMP is mandatory. Deviation from this plan may find you personally liable and subject to further action.**



Construction / decommissioning vehicle routes - egress:

From the site, right turn onto Wollara Road northbound, continue straight onto Ringwood Road northbound, left turn onto Golden Highway westbound, right turn onto Barnett Street northbound, U-turn via turnaround facility approximately 190 metres north and continue onto Barnett Street southbound, left turn on Golden Highway eastbound.

Operational vehicle routes - egress:

Vehicles can undertake all legal movements associated to their vehicle type.

**VEHICLE MOVEMENT PLAN – GOULBURN RIVER SOLAR FARM  
SITE EXIT / EGRESS**

**Adherence to this VMP is mandatory. Deviation from this plan may find you personally liable and subject to further action.**

## Appendix H Stakeholder Consultation

Comments were received from TfNSW on 21 October 2024, Midwestern Regional Council on 1 October 2024 and Upper Hunter Shire Council on 21 October 2024 following review of the Goulburn River Solar Farm – Traffic Management Plan, Lightsource bp have addressed each comment with responses provided in **Table 1** below.

**Table 1 Responses from TfNSW, Mid-Western Regional Council and Upper Hunter Shire Council**

TfNSW Comment	Lightsource bp's Response
<p>Condition B10 (b) has not been satisfied and is required to include details of the road works required for the sealing of Barnett Street.</p>	<p>a. <i>Section 2.4, dot point 7 of this TMP</i></p> <p><b>New Text has been added to this TMP:</b></p> <p>Road upgrades required on Ringwood Road, Wollara Road and at the intersections of Ringwood Road <b>and Barnett Street</b> with the Golden Highway.</p> <p>b. <i>Section 3.1.3 Intersection Upgrade – Golden Highway &amp; Ringwood Road</i></p> <p><i>“The Golden Highway / Ringwood Road intersection .....</i>  <i>The intersection upgrade footprint is shown in <b>Figure 3-3</b>. Drawings of the proposed intersection upgrades are provided in <b>Appendix B</b>.</i></p> <p>c. <i>Rename Section 3.1.4 Intersection Upgrade – Golden Highway &amp; Barnett Street</i></p> <p><b>New Text has been added to this TMP; Section 3.1.4 Intersection Upgrade – Golden Highway and Barnett Street :</b></p> <p>The Golden Highway / Barnett Street intersection will be upgraded to accommodate the requirements for the Barnett Street turnaround point. These upgrades will include:</p> <ul style="list-style-type: none"> <li>• Reconstruction of the initial 30m of the throat of Barnett Street to a TfNSW compliant sealed pavement, with unsealed shoulders-</li> <li>• Delineation, line-marking and give-way signage, within Barnett Street.</li> </ul> <p>Installation of truck turning movement warning signs on both approaches to the intersection.</p> <p>The intersection upgrade footprint is shown in <b>Figure 3-4</b>. Drawings of the proposed intersection upgrades are provided in <b>Appendix B</b>.</p>

	<p>We will also include the 80% design drawing for Barnett St intersection, currently being assessed by TfNSW as part of WAD application.</p>
<p>Condition B10 (d) requires greater controls to be implemented to ensure compliance. This should include commitment to surveillance and records to be kept and available for auditing.</p>	<p><b>New text has been added to this TMP; Section 5.6 Management of Construction Vehicle Routes:</b></p> <ul style="list-style-type: none"> <li>• The construction vehicle route applies to all vehicles associated with the development, including through early works, road upgrades, construction, upgrading and decommissioning.</li> <li>• To ensure that construction vehicle drivers adhere to the left-in, left-out arrangement at the Golden Highway / Ringwood Road intersection during construction, the following will be implemented:             <ul style="list-style-type: none"> <li>• A monitoring camera will be installed at the Golden Highway/Ringwood Road intersection:                 <ul style="list-style-type: none"> <li>▪ prior to construction commencing</li> <li>▪ capturing footage from one hour prior through to one hour post approved construction hours (i.e., from 0600 to 1900)</li> <li>▪ footage will be reviewed weekly for months one and two of construction, then monthly thereafter or in response to a complaint</li> <li>▪ the monitoring approach will be assessed for effectiveness periodically, and any proposed changes discussed proactively with TfNSW.</li> <li>▪ Appropriate corrective measures</li> </ul> </li> <li>• Installation of additional signage on Ringwood Road stating that “site traffic must turn left”.</li> <li>• The construction contractor will undertake spot checks at the intersection for compliance. This will involve counts of vehicles leaving site, coupled with vehicle counts at the turn-around bay on Barnett Street. A randomised spot check will occur monthly at a minimum, or in response to a complaint from the community or feedback from within the Project team.</li> </ul> </li> <li>• A register of heavy traffic movements in and out of the Project Area will be maintained during construction. The register will be maintained by the Construction Contractor.</li> <li>• The approved vehicle route will be reinforced during site inductions, daily toolbox talks and as part of the Driver Code of Conduct.</li> <li>• The approved vehicle route will be specified in a pre-arrival flyer which would detail delivery instructions and be distributed to drivers before coming to site.</li> <li>• Quarterly Inspections and routine audits of the transport route shall be conducted to ensure compliance to site requirements on Traffic Management.</li> </ul> <p>There is potential for limited deliveries/supplier movements from adjacent LGAs / areas to the west of the Project Site, with a number of companies based to the west of the Project Site expressing interest in sub-contracting or supplying aspects of the Project. In this instance, adjacent LGA road networks will be utilised, however transport movements will align with the conditions of consent (B3-B5) and approach the site from the east, along the Golden Highway.</p>

	<ul style="list-style-type: none"> <li>• Only locally based workers in light vehicles will be able to use Wollar Road to the south of the Project Site, with agreement from the Construction Contractor.</li> <li>• Limited traffic movements from suppliers operating under their own permits/licences (i.e., Environmental Authority) may utilise an alternative traffic route (where this is permitted under their licence conditions), with agreement from the Construction Contractor. These movements will be recorded as part of the register of heavy traffic movements in and out of the Project Area (maintained by the Construction Contractor).</li> </ul>
<p>Condition B10 (e)(x) is not satisfied suitably. Limited detail is provided regarding pick up/ drop off locations and further information is required on how uptake of the shuttle bus will be enforced</p>	<p><b>3.6.4 Employees Shuttle Bus Service</b></p> <p>As noted in <b>Section 3.6.3</b>, a shuttle bus service will be commissioned to assist with employee and contractor transportation during construction to the Project. An estimated 30 one-way shuttle bus journeys (15 return trips) will transport locally-based workers to and from the Project Area. Shuttle buses will also transport workers staying at the TWA Facility into Merriwa to replenish personal supplies and for general recreation.</p> <p>Within the Project Area, shuttle buses will transport workers from the TWA Facility to construction compounds across the Project. This service is intended to manage vehicle traffic on the local road network as well as internally within the Project Area.</p> <p>The Merriwa pick up/ drop off point would be located either on the Golden Highway or on Vennacher Street, close to the intersection with the Golden Highway, as both locations have parallel parking spaces. This location is near the Merriwa Post Office. The shuttle bus may be used to provide workers with connections to the local bus services, for travel to other regional locations such as Scone, Dubbo and Newcastle.</p> <p>The nominated location is also in close proximity to the local key Merriwa town centre commercial businesses (i.e., IGA supermarket).</p> <p>Shuttle bus services have been successfully used on recent Lightsource bp projects, Wellington Solar Farm and Wellington North Solar Farm.</p> <p>Measures to actively encourage use of shuttle buses will include:</p> <ul style="list-style-type: none"> <li>• Employees will be made aware of the contractual requirements and actively encouraged to use the shuttle bus service. This will be managed through incorporation into site inductions and regular broadcasting (e.g. posters and signage throughout the TWA Facility).</li> <li>• Use of the shuttle service (and ride-sharing) will be contractually required (see also Section 3.6.3).</li> <li>• Provision of a certain number of on-site car parking spaces will be monitored through ad hoc visual inspections to identify whether increased parking demand has resulted from reduced usage of the shuttle bus service and/pr carpooling.</li> </ul>

	<ul style="list-style-type: none"> <li>Secure storage would be available on-site to trades for safely storing tools/ equipment, which would otherwise necessitate a higher rate of workers driving to site.</li> </ul>
<p>Condition B10 (e)(xi) is not satisfied suitably. Further information is required on how light vehicle numbers will be limited during construction</p>	<p>During peak construction and with the on-site TWA Facility operational the following trips are anticipated to be generated by the Project:</p> <ul style="list-style-type: none"> <li>Light vehicles – there are up to approximately 60 two-way movements expected per day (120 light vehicle movements per day), as estimated in the amended TTIA (Turnbull 2023).                         <ul style="list-style-type: none"> <li>Contractually, car-pooling and ride-sharing initiatives will be required and encouraged with the Project engaged subcontractors and suppliers, for their Workers. This will be undertaken by shuttle busses, which are included in the light vehicle volume.</li> </ul> </li> <li>Shuttle buses – 15 two-way movements expected per day (30 shuttle bus movements per day), as estimated in the Amended TTIA (Turnbull 2023).</li> <li>Heavy vehicles – 55 movements a day (a maximum of 15 heavy vehicle movements per hour) during construction, upgrading or decommissioning.</li> </ul> <p>24 movements of heavy vehicles requiring escort during construction, upgrading or decommissioning.</p>
<p>Condition B10 (e)(xii) is not satisfied suitably. Further information on how heavy vehicles would be scheduled to avoid convoys and light vehicles is required. An indicative schedule of movements of heavy vehicles at peak construction is required.</p>	<p><b>New text has been added to this TMP; S 5.5 (Driver Code of Conduct)</b></p> <p>As per NSW Road Rules (2014) 127 (1), a driver of a long vehicle (7.5m long or longer), except on a multi-lane road or any length of road in a built-up area, <u>must</u> travel at least 60m behind any other long vehicle, other than a road train.</p> <p>The potential for heavy vehicles to convoy or platoon together will be addressed through direction by the Construction Contractor to subcontractors and suppliers. Strategies will include:</p> <ul style="list-style-type: none"> <li>Staggered Scheduling: Heavy vehicle movements will be staggered to prevent convoys and minimise traffic congestion, particularly during peak times.</li> <li>Off-Peak Scheduling: Heavy vehicles will operate during off-peak hours to reduce interactions with peak traffic periods on the Golden Highway.</li> <li>Break Time Coordination: Managing driver breaks to avoid clustering after rest periods.</li> </ul> <p>All long vehicle drivers and their respective supervisors/ transport managers will be reminded that:</p> <ul style="list-style-type: none"> <li>There is a maximum of 55 (two-way trips; 110 one-way trips) heavy vehicle movements per day, and a maximum of 15 two-way heavy movements per hour.</li> </ul>

	<ul style="list-style-type: none"> <li>• when heavy vehicle drivers are leaving their depot/ quarry/ batch plant, or the Project site, that a minimum 1 minute gap in departure times is provided.</li> <li>• when heavy vehicle drivers stop for breaks (rest, smoko, lunch, etc) together, that on departure from the stop that each subsequent heavy vehicle allows at least 1 minute between each subsequent heavy vehicle departure from the stop.</li> </ul> <p>The Driver Code of Conduct will be communicated to all personnel through site inductions, toolbox talks and pre-start meetings. Any non-compliance with the Drivers Code of Conduct will be managed through the EPC's general business code of conduct, and the Contracts that are in place with their subcontractors and suppliers.</p> <p><b>Appendix F, Section 1 – General Commitments = new text</b></p> <p>Drivers must:</p> <ul style="list-style-type: none"> <li>• Comply with the Project Traffic Management Plan.</li> <li>• Hold the appropriate licence class for the vehicle being driven.</li> <li>• Comply with road laws and regulations.</li> <li>• Exercise extra care in adverse weather conditions that may affect road safety such as driving at night, fog, dust, wet weather and flooding.</li> <li>• Report any observations of driver fatigue or misconduct to the HSE Coordinator.</li> <li>• Respect the rights of all road users to share the road.</li> <li>• Cover loads when transporting loose materials (e.g. aggregate).</li> <li>• Avoid travelling in convoys or platoons, with all long vehicles (7.5m long or longer) to be driven at least 60m behind any another long vehicle that they are following.</li> <li>• Report any incidents in accordance with the Work Health and Safety Management Plan.</li> </ul>
<p><b>Upper Hunter Shire Council</b></p>	
<p>We are generally satisfied with the TMP, however, Council does have some concerns in relation to the sizing of the proposed culverts on Ringwood Road identified in the proposed road and culvert upgrades. It is understood that the designs are inconsistent with the previously agreed design parameters. Council is currently working with your design engineers to address this issue.</p>	<p>Lightsource bp will continue to work with UHSC to confirm design suitability prior to commencing road and culvert upgrades on Ringwood and Wollara Roads.</p>

### Mid-Western Regional Council

**Council notes:**

- The only road within the Mid-Western Region (MWR) LGA affected by the proposal is Wollar Road, which is assessed as carrying light vehicle commutes to the construction site only.
- There is no truck or specialist heavy haul traffic listed using roads in the MWR LGA
- There are no intersections listed in the TMP requiring capacity assessment.

Council requests, if the TMP changes, that Council are consulted.

No changes required.

Lightsource bp will consult with Mid-Western Regional Council if any changes to the TMP are proposed.